

Exception Request No.: 9
Section: WBR3
Station: 2492+70 to 2499+71±
Drawing No.: WBR3 C219 to 220
Survey Report Cross Reference No.: WBR3 C215 to C216
Exception Type: Alignment in Pavement
Splice Enclosure in Pavement

Summary of Justification for Exception

NPT is requesting an exception from the UAM guidelines for the location of the cable trench in the pavement on US 3, Daniel Webster Highway from STA 2492+70 to 2499+71± of the NPT WBR3 underground alignment, Sheets C219-C220 and the splice enclosure at STA 2499+50±. The proposed alignment and splice enclosure are set within the pavement to avoid conflicts with existing utilities and drainage systems located on both sides of the road.

Technical Discussion of Justification of Exception

Alignment in Pavement/Splice Enclosure in Pavement

The positioning of the alignment and splice enclosure are constrained by utilities on both the eastern and western sides of US 3. A list and discussion of each of the existing utilities is provided below.

1. A 12-inch clay sanitary sewer main runs along the edge of pavement on the east edge of the road. Relocating the sewer to provide additional space for the proposed alignment outside the pavement will result in extensive additional pavement impacts and will require the sewer to be installed in the eastbound travel lane, resulting in additional piping and sewer manhole relocations.
2. A 6-inch water main runs parallel to and just inside the edge of pavement on the west side of the road. Relocating the water main will result in pavement impacts and traffic impacts because the existing water main is in the southbound travel lane.
3. A storm drain system including piping and drainage structures runs along the west edge of the road. The storm drain piping and drainage structures are based on existing drainage patterns and cannot be relocated without modifications to the existing road.
4. An existing overhead distribution line runs along the east side of the ROW. Relocating the utility pole (NHECO 10-36) to allow room to move the alignment outside the pavement and to the east of the sewer will require modifications to the structure guying and anchoring, which is located outside of the ROW on private property.

The splice enclosure at Station 2499+50 is positioned to align with the start of the HDD installation at that location. As a result, any alignment change to the west side of the road would need to return to the east side to meet the HDD alignment, resulting in two additional road crossings (and requiring DOT exception approval of these crossings).

Excavation limits and work areas are shown on the attached drawings. During construction, one lane will remain open to traffic at all times.

Impacts

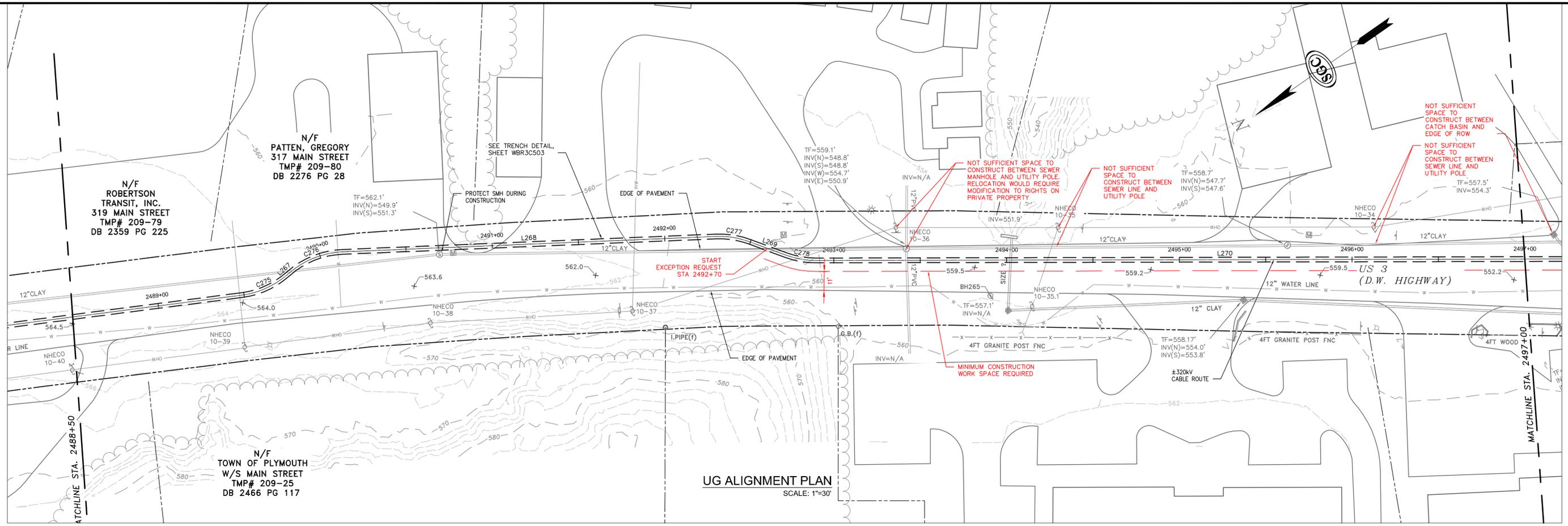
Alignment/Splice Enclosure in Pavement

The design, as proposed, will not adversely affect the design, construction, stability, traffic, safety, environmental commitments, maintenance, or operation of the highway. The alignment has been located 5-feet from the edge of the existing sewer main to avoid future conflicts with sewer repairs or replacement or disruption to the existing sanitary system. The ductbank/splice enclosure and pavement restoration will be designed and constructed in accordance with conditions outlined in the NHDOT's April 3, 2017 letter to the New Hampshire Site Evaluation Committee. The proposed splice enclosure will be of a minimum rating of HS-20, in accordance with NHDOT requirements. The installation's proposed depth meets NHDOT's criteria relating to the structural box to minimize any potential conflicts with maintenance and future highway projects. A traffic control plan has been submitted to the NHDOT for this design and complies with the Manual on Uniform Traffic Control Devices.

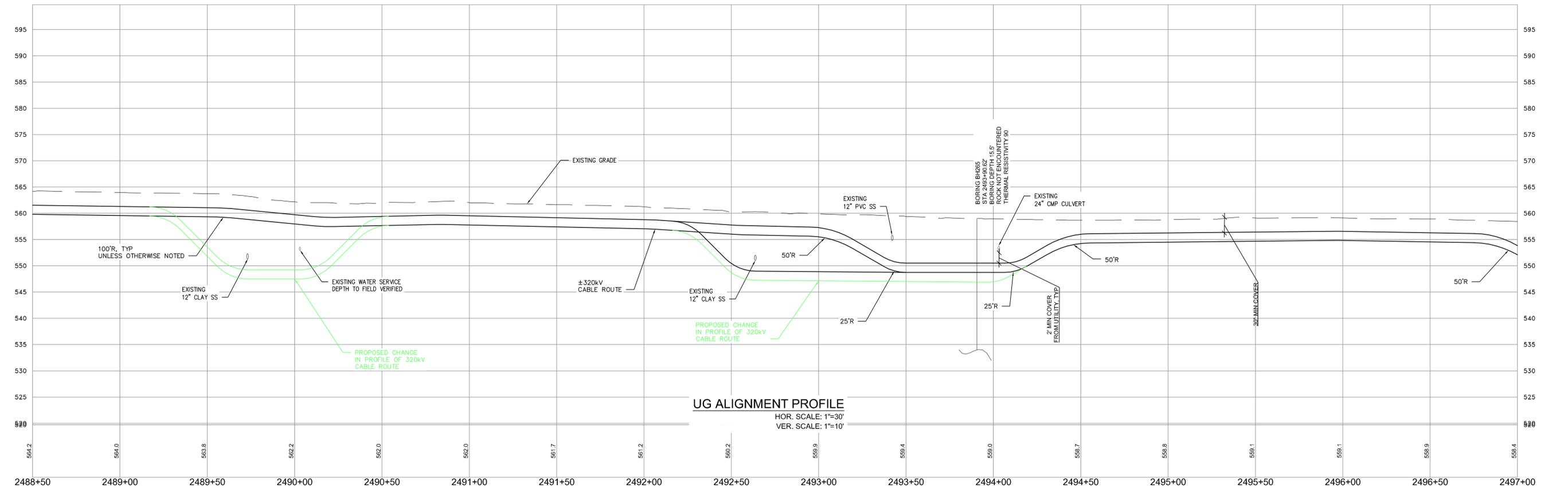
Supporting Documentation

See attached Exhibit A showing a plan and profile view for the proposed installation.

PRELIMINARY - NOT FOR CONSTRUCTION



UG ALIGNMENT PLAN
SCALE: 1"=30'



UG ALIGNMENT PROFILE
HOR. SCALE: 1"=30'
VER. SCALE: 1"=10'

NO.	REVISION	DATE	BY	CHKD.	APPV.
0	EXCEPTION REQUEST	05/02/17	TOD	DMW	CHD



Transmission Business

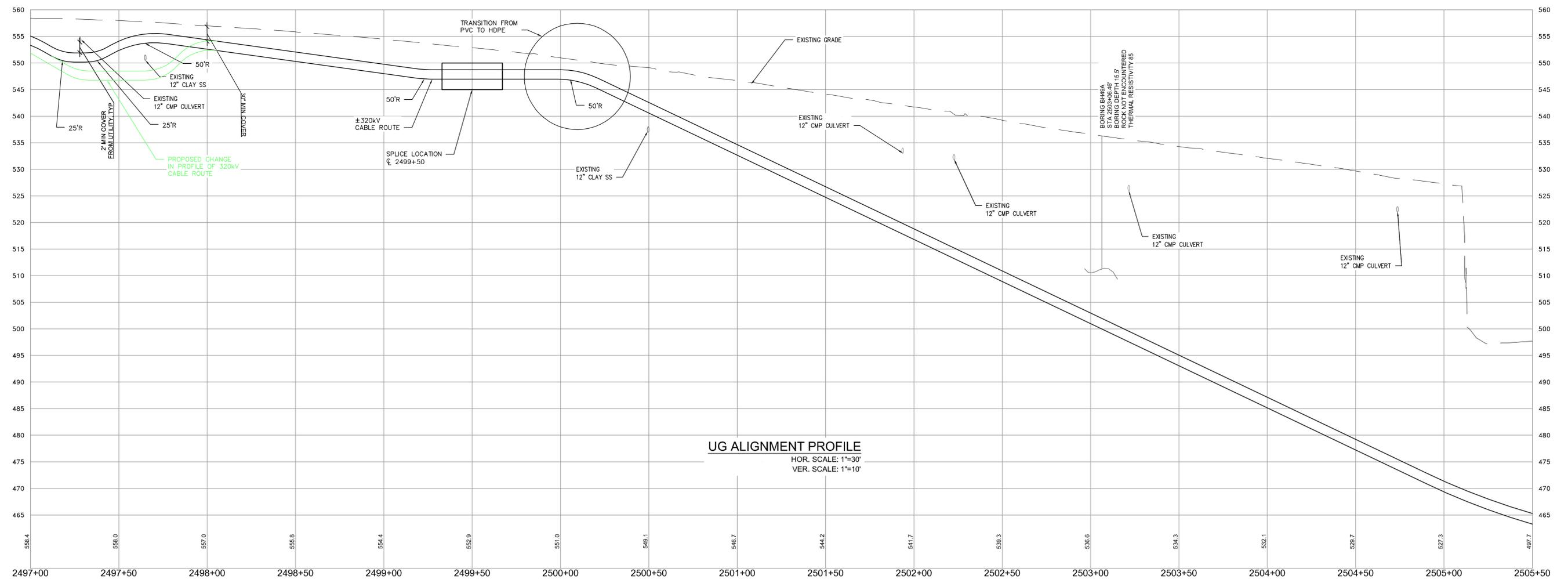
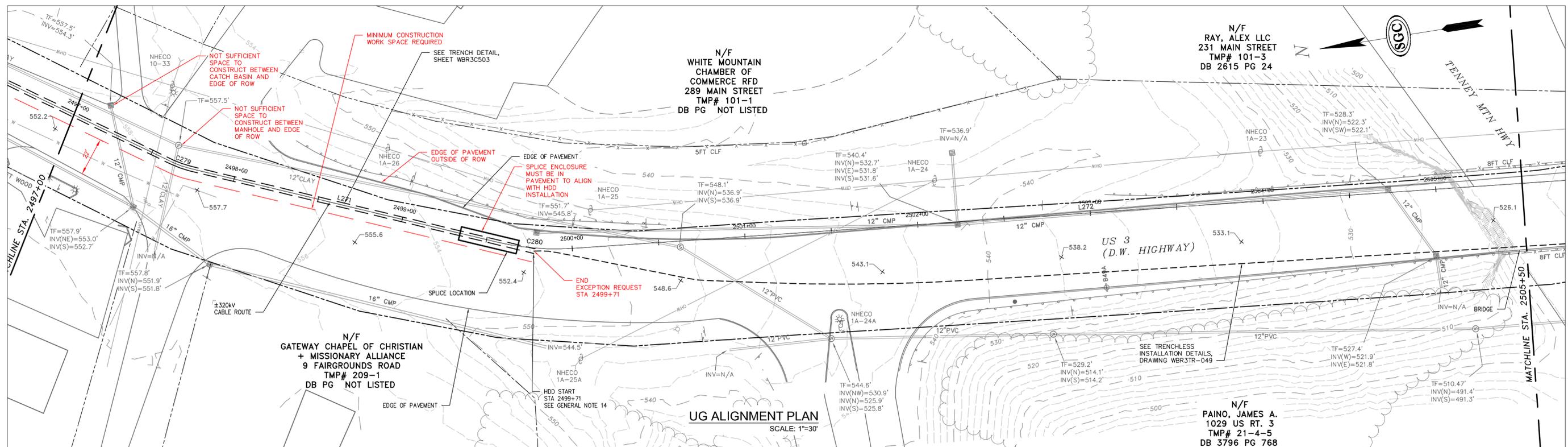
EXCEPTION 9 - ALIGNMENT IN PAVEMENT AND SPACE ENCLOSURE IN PAVEMENT; NPT WBR3-UNDERGROUND ALIGNMENT WBR3 SECTION-STA. 2492+70 TO STA. 2499+71
DATE: 04/20/17

TRANSMISSION LINE: WBR3

EXHIBIT A.1

11"X17" PLOT
PLAN SCALE: 1"=60'

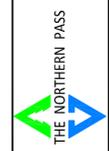
PRELIMINARY - NOT FOR CONSTRUCTION



UG ALIGNMENT PROFILE
 HOR. SCALE: 1"=30'
 VER. SCALE: 1"=10'

11"X17" PLOT
 PLAN SCALE: 1"=60'

NO.	DATE	BY	CHKD	APPRV.	REVISION
0	05/02/17	TDD	DRWN	CHD	APPRV.



Transmission Business

EXCEPTION 9 - ALIGNMENT IN PAVEMENT AND SPICE ENCLOSURE IN PAVEMENT; NPT WBR3-UNDERGROUND ALIGNMENT WBR3 SECTION-STA. 2497+00 TO STA. 2499+71
 DATE: 04/20/17
 TOWN: PLYMOUTH

TRANSMISSION LINE:
WBR3

EXHIBIT A.2