

Exception Request No.: 67 (Rev 1)
Section: WBR3
Town: Thornton
Highway: US 3 (Tier 2)
Station: 1793+40 to 1797+30
Drawing No.: WBR3 C137
Survey Report Reference No.: WBR3 C133
Exception Type: Alignment in Pavement

Traffic Information

NHS: No
ADT: 955
Traffic Control Type: Alt 1-way
Traffic Control Duration: Traffic control duration is estimated to be 7 days for the proposed installation. If the requested exceptions for the alignment in pavement are not granted, NPT expects an additional 6 days of work requiring traffic control.

Summary of Justification for Exception

NPT is requesting an exception from the UAM guidelines for the location of the cable trench in the pavement on US 3, Daniel Webster Highway from station 1793+40 to 1797+30 of the NPT WBR3 Underground Alignment. (See Exhibit A.)

Due to limited ROW space outside the pavement and beyond the existing guardrail, construction outside the guardrail is not practicable because: (i) if the guardrail is not removed, NPT does not have the necessary property rights to construct outside the NHDOT ROW; (ii) if the guardrail and a portion of the roadway is temporarily removed to allow construction of the duct bank in the slope without extending past the right of way limits for benching, the traffic impacts and costs of this construction method are greater than the proposed installation. The proposed alignment is located beneath the pavement at a 5-foot offset from the guardrail consistent with NHDOT's request to avoid future conflicts with guardrail repairs or replacement or disruption to the existing guardrail system.

Technical Discussion of Justification of Exception

The roadway alignment at this location is constrained by guardrail on the eastern side of US 3 with moderate to steep slopes on the outside of the guardrails. (See Exhibits A and B.) Consequently, the steep slopes behind the guardrail, combined with NHDOT's requested offset of 5-feet from the existing guardrail, would result in significant constructability issues (if the guardrail were not removed), including the need for benching into the side slope to create a level and safe working area. The modified side slopes would extend beyond the ROW limits. (See Exhibit B.)

At the request of the DOT, we also evaluated an option to remove the guardrail and a portion of the roadway to allow NPT to construct the duct bank in the slope without extending past the right of way limits. Considerable amounts of materials would have to be removed and transported to another site for temporary storage in order to bench into the slope. These materials would then have to be transported back to restore the site after the duct bank was completed. (See Exhibit C.) This option would significantly increase the time necessary in the NH DOT ROW required to construct the ductbank

and would be more costly, having a net increase of \$52,621 including the cost of material transport and new guardrail installation. (See Exhibit D.) (Note: This marginal cost estimate does not factor in the potential that native materials cannot be used during reburial because more expensive, select materials may be needed to address cable thermal issues.) In addition, traffic impacts would be significantly greater for this option (as compared to the proposed installation) due to the additional work for the benching activities.

Additionally, NPT has liability concerns regarding DOT's request that NPT install new guardrails after completion of its work. Unlike NHDOT, if NPT were to install new guardrails, NPT would not have the benefit of immunity protections afforded to NHDOT under New Hampshire law. See N.H. R.S.A. § 230:80. Therefore, even in cases where NPT deemed the cost of the "guardrail replacement option" to be a reasonable project cost for a particular location, NPT could not agree to have any role in work to replace the guardrails unless NHDOT were willing to agree to defend, indemnify, and hold harmless NPT against any and all claims related in any manner to, or arising out of, the installation of the new guardrails. If NHDOT were not willing to provide such protection to NPT, then NPT would be willing, in the alternative, to reimburse NHDOT for the cost NHDOT and/or its contractors incur to replace any guardrails removed during our work, but NPT could not have any role in such work. However, NPT is not requesting the "guardrail replacement option" at this location, where it deems the additional traffic impacts and cost of this work to be prohibitive.

We evaluated placing the cable trench alignment along the west side of US 3, opposite the guardrail. Additional constraints existing on the west side include steep slopes starting at the edge of pavement and would also require two additional road crossings. The proposed location although in the pavement provides the least pavement and traffic impact during construction.

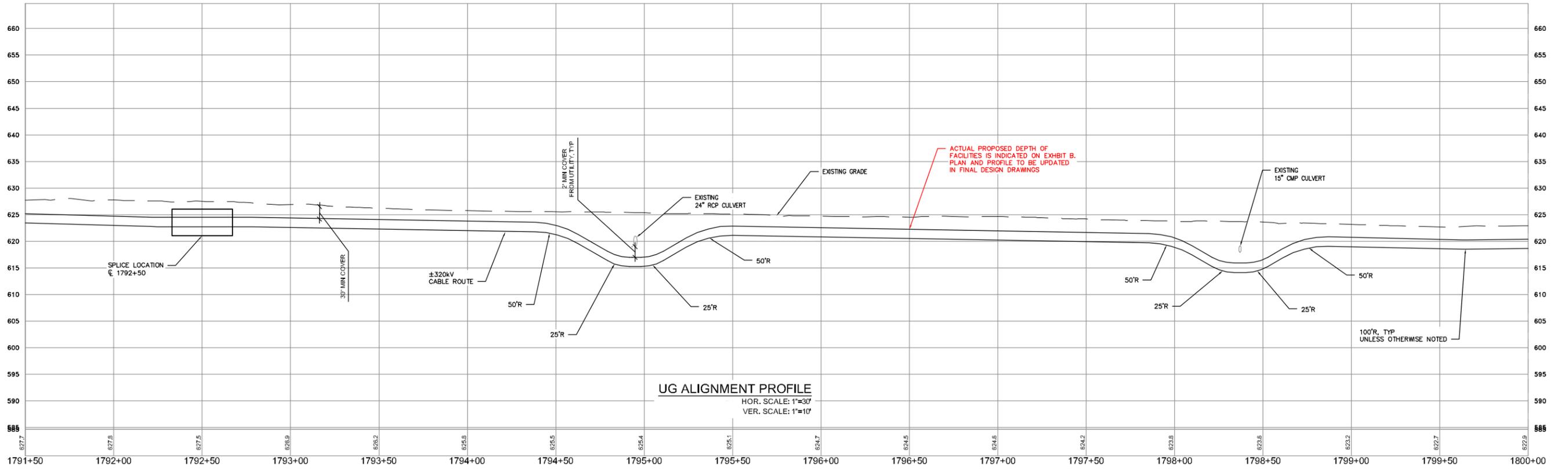
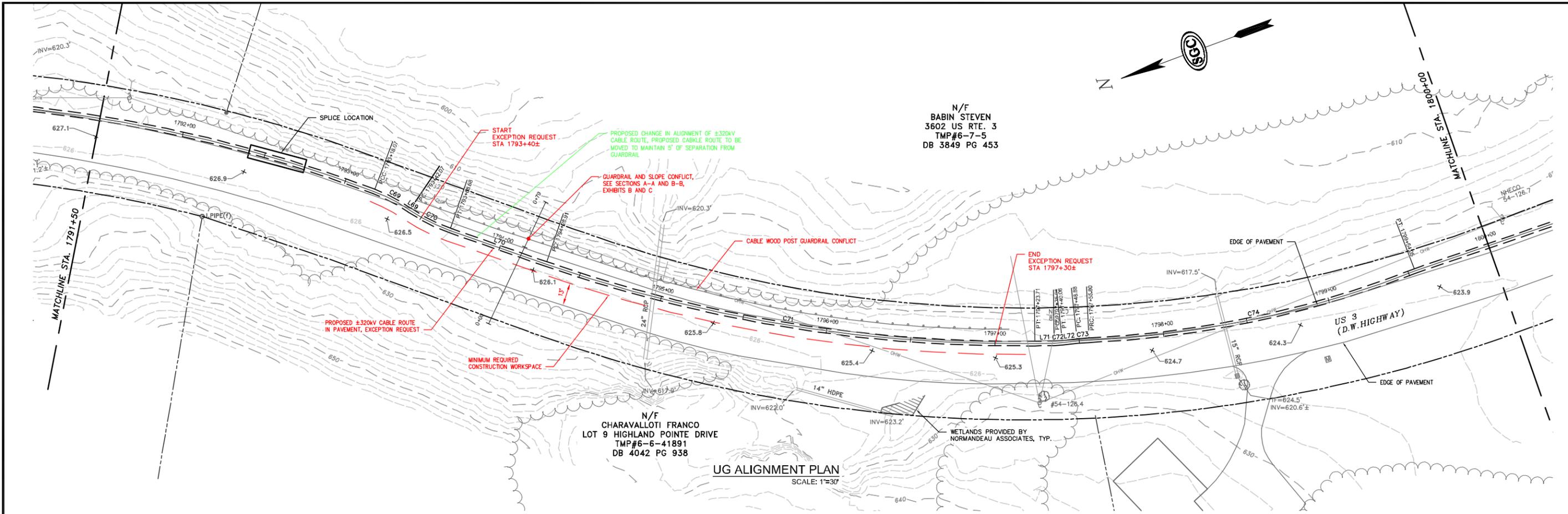
Impacts

The design, as proposed, will not adversely affect the design, construction, stability, traffic, safety, environmental commitments, maintenance, or operation of the highway. The alignment has been located 5-feet off the edge of the guardrail, to avoid future conflicts with guardrail repairs or replacement or disruption to the existing guardrail system. The installation of the ductbank and pavement restoration will be designed and constructed in accordance with conditions outlined in the NHDOT's April 3, 2017 letter to the New Hampshire Site Evaluation Committee. The installation's proposed depth meets NHDOT's criteria relating to the structural box to minimize any potential conflicts with maintenance and future highway projects. A traffic control plan has been submitted to the NHDOT for this design and complies with the Manual on Uniform Traffic Control Devices.

Supporting Documentation

See attached Exhibits A, B, and C showing a plan, profile, and sections for the proposed installation and alternatives, and see Exhibit D for cost estimates.

PRELIMINARY - NOT FOR CONSTRUCTION



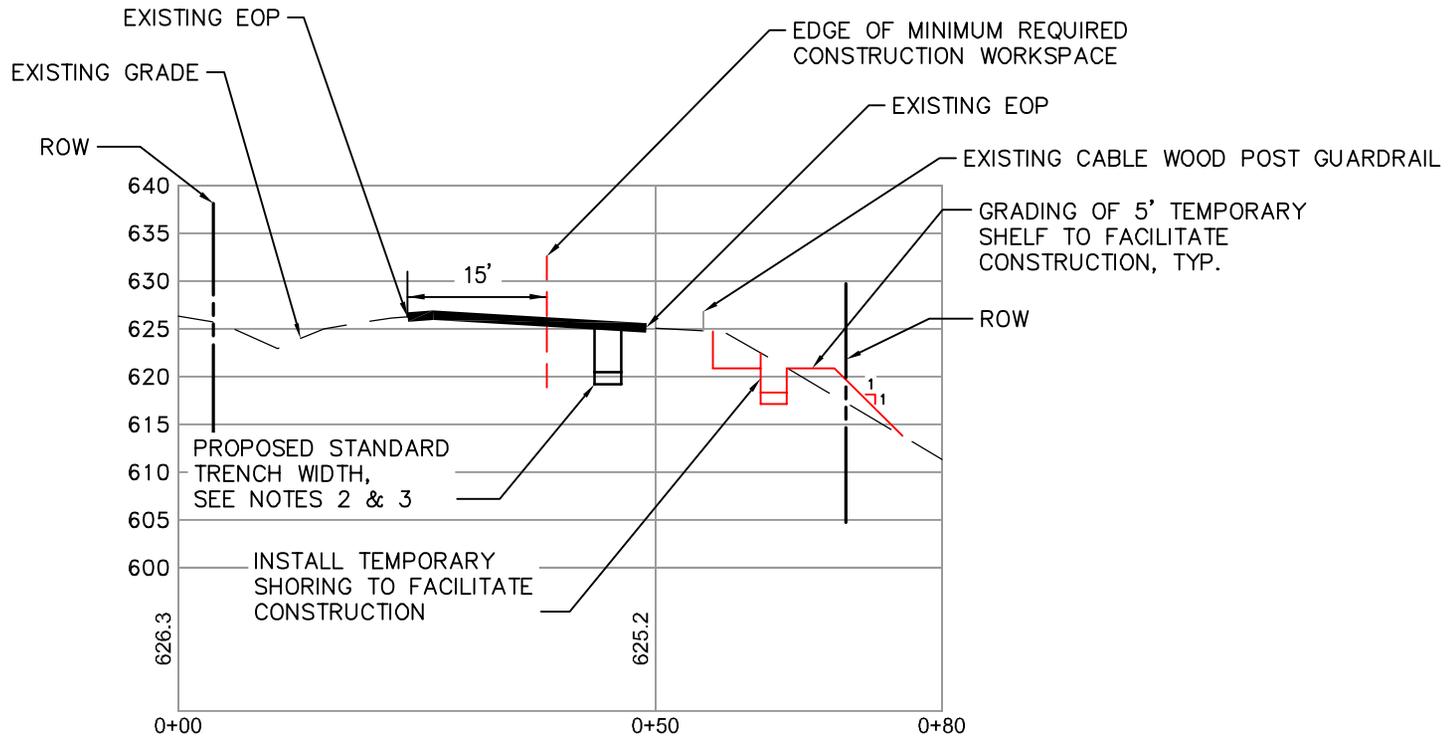
NO.	DATE	REVISION	BY	CHKD	APPV.
0	06/27/17	ISSUE FOR PERMIT	MRR	THM	



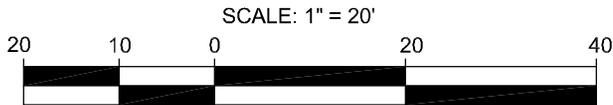
Transmission Business

EXCEPTION 67-ALIGNMENT IN PAVEMENT
NPT WBR3-UNDERGROUND ALIGNMENT
WBR3 SECTION-STA. 1793+40 TO STA. 1797+30
SCALE: H. 1"=30', V. 1"=10'
DATE: 05/20/17

TRANSMISSION LINE:
WBR3
EXHIBIT A



SECTION A-A
SCALE: 1"=20'



NOTES:

1. TRENCH LOCATION SHOWN IN RED IS NOT PROPOSED AND ONLY INTENDED TO DEMONSTRATE CONSTRUCTABILITY ISSUES.
2. TRENCH WIDTH SHOWN TO BE MAINTAINED USING TRENCH JACKS AND SHEETING.
3. PROPOSED CABLE ROUTE TO BE MOVED CLOSER TO EDGE OF PAVEMENT AND MAINTAIN 5 FEET OF SEPARATION FROM THE GUARDRAIL. CHANGE IN ALIGNMENT WILL PROVIDE ADDITIONAL ROOM FOR WORKSPACE AND MAINTENANCE OF TRAFFIC

JOB NO.: 1384001

TITLE:
EXCEPTION 67
ALIGNMENT IN PAVEMENT
NPT-WBR3 UNDERGROUND ALIGNMENT
WBR3 SECTION-STA 1793+40 TO 1797+30±
TOWN: THORNTON

PREPARED FOR:
NH DOT
7 HAZEN DRIVE
CONCORD, NH

REVISIONS:

NO.	DATE	EXCEPTION REQUEST
0	06/21/2017	



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Tel: 207-217-6799
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14 School Street, Suite 203-A
Bristol, VT 05443
Tel: 802-736-9296

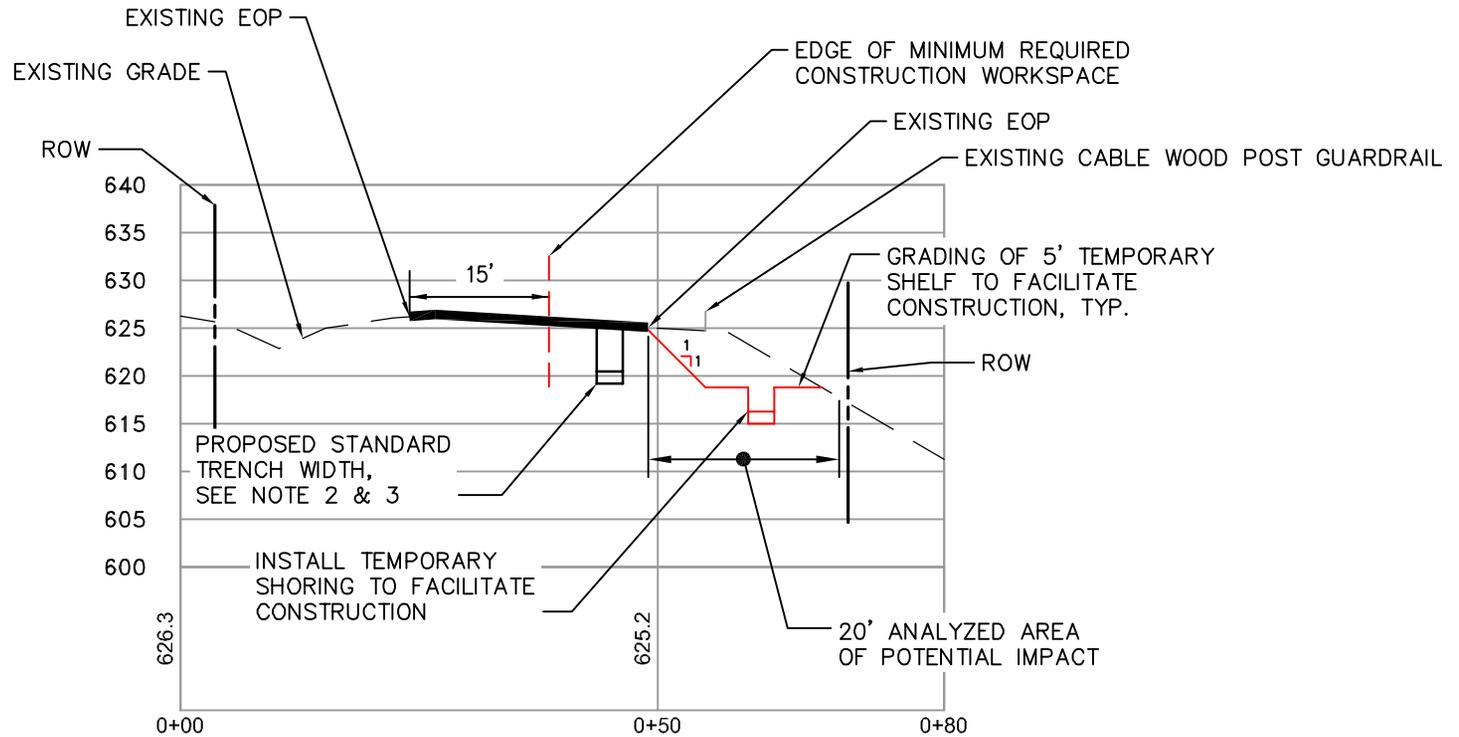
Galinda Tower 1, Suite 2478
2700 Post Oak Boulevard
Houston, TX 77056

EXHIBIT NO.: B

DATE: 05/2017

DRAWN: MRR

SCALE: 1" = 20'



GUARDRAIL SECTIONS

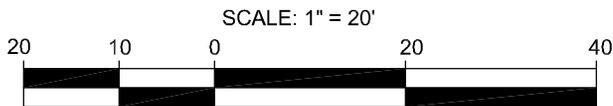
START STATION	END STATION	LENGTH (FT)
1793+50	1797+10	360

ALTERNATE LOCATION CUT VOLUME EVALUATION:

1. GUARDRAIL LENGTH: 360 FT
2. APPROX. CUT AREA: 62 SF
3. APPROX. CUT VOLUME: 22,320 CF (827 CY)

SECTION B-B

SCALE: 1"=20'



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PREPARED FOR:
 NH DOT
 7 HAZEN DRIVE
 CONCORD, NH

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 Galleria Tower 1, Suite 2478 2700 Post Oak Boulevard Houston, TX 77056

EXHIBIT NO.: C

DATE: 05/2017

DRAWN: MRR

SCALE: 1" = 20'

Exhibit D - Exception 67 Cost Estimates

Additional Cost for Removing Guardrail and Benching into slope

Length	360			
Cut Volume	827			
	Quantity	Units	Unit Price	Total
Material Removal, Hauling & Replacement	827	CY	\$42.19	\$34,891.13
Guardrail	360	LF	\$49.25	<u>\$17,730.00</u>
Net Additional Cost				\$52,621.13

1. Cost assumes rock excavation not required.
2. Cost assumes off site storage available within 20 miles