

Exception Request No.: 185
Section: NRTH
Town: Stewartstown
Highway: Bear Rock Road (Tier 4)
Station: 323+40± to 323+70±
Drawing No.: NRTH C138
Survey Report Cross Reference No.: NRTH-38

Exception Type: Alignment in Pavement (Road Crossing)

Traffic Information

NHS: No
ADT: Information not available on NH DOT website
Traffic Control Type: Alt 1-Way
Traffic Control Duration: Traffic control duration is estimated to be 5 days for the proposed installation, during which one lane will be closed.

Summary of Justification for Exception

NPT is requesting an exception from the UAM guidelines for the location of the cable trench in the pavement on Bear Rock Road from approximately station 323+40 to 323+70 of the NPT NRTH Underground Alignment. Due to limited ROW space outside the pavement and beyond existing utility poles, construction outside the paved area is not practicable because NPT does not have the necessary property rights to construct outside the NHDOT ROW on private property. The proposed alignment crosses Bear Rock Road to avoid the utility poles and keep more of the alignment out of the pavement. The attached Exhibit A has been provided to demonstrate that crossing the road will allow for more of the alignment to be installed outside the pavement.

Technical Discussion of Justification of Exception

The proposed alignment is within the roadway because of constraints posed by existing utility poles and limited space outside the NHDOT ROW on the north side of Bear Rock Road.

An existing overhead distribution line runs along the north side of the ROW on Bear Rock Road. Relocating the utility poles to provide sufficient area to construct the duct will not provide a sufficient area between the edge of pavement and the NHDOT ROW limits to install the cable trench or will require additional rights for the installation of guys and anchors on private property outside the NHDOT ROW. Crossing the road to the south side will avoid the utility poles and allow for more of the alignment to be installed outside the pavement, with the exception of the crossing.

Excavation limits and work areas are shown on the attached drawings. (See Exhibit A.) During construction, one lane will remain open to traffic at all times.

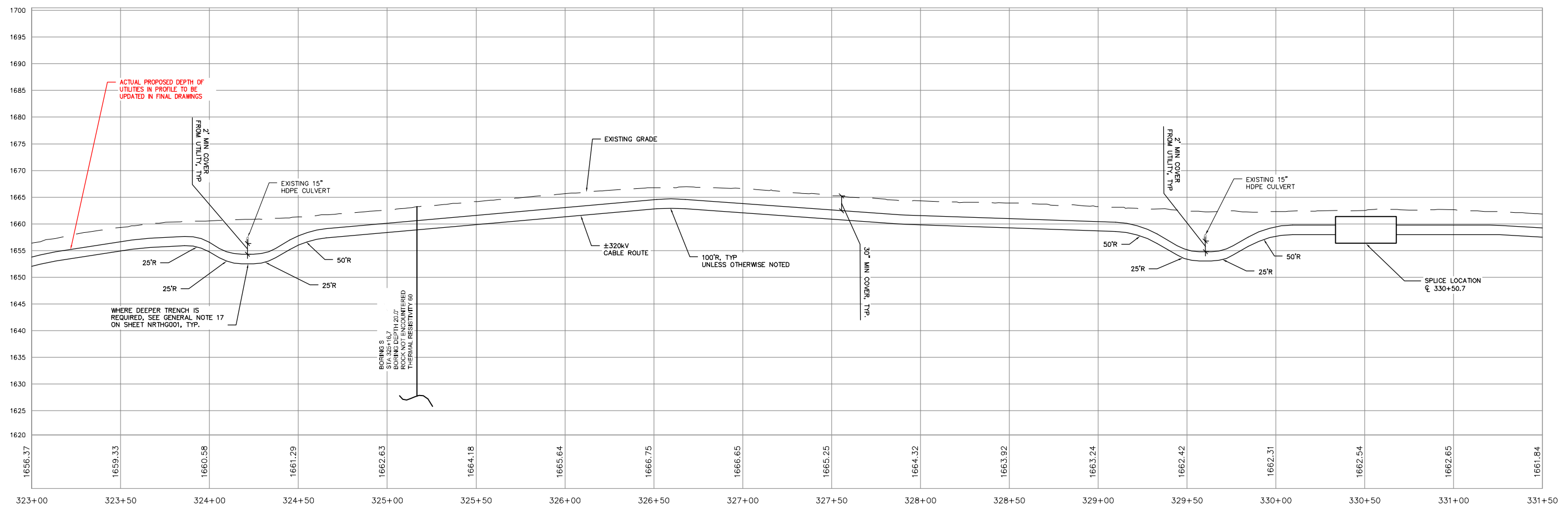
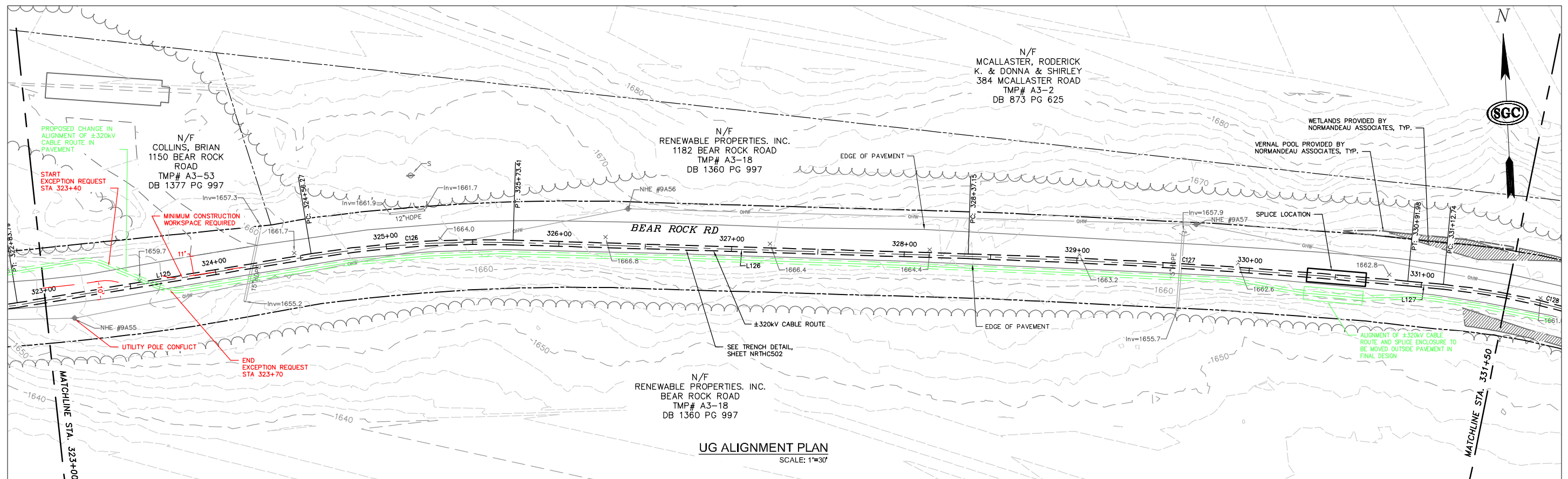
Impacts

The design, as proposed, will not adversely affect the design, construction, stability, traffic, safety, environmental commitments, maintenance, or operation of the highway. The alignment has been located 5-feet off the edge of the existing utility poles, to avoid future conflicts with repairs or replacement. The installation of the duct bank and pavement restoration will be designed and constructed in accordance with conditions outlined in the NHDOT's April 3, 2017 letter to the New Hampshire Site Evaluation Committee. The installation's proposed depth meets NHDOT's criteria relating to the structural box to minimize any potential conflicts with maintenance and future highway projects. A traffic control plan has been submitted to the NHDOT for this design and complies with the Manual on Uniform Traffic Control Devices.

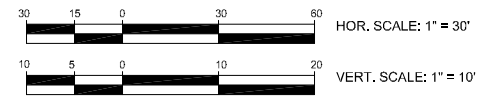
Supporting Documentation

See attached Exhibit A showing a plan and profile.

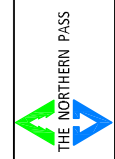
PRELIMINARY - NOT FOR CONSTRUCTION



UG ALIGNMENT PROFILE
HOR. SCALE: 1"=30'
VER. SCALE: 1"=10'



NO.	DATE	REVISION	DRWN	CHKD	APPRV.
0	06/21/17	EXCEPTION REQUEST			



Transmission Business

EXCEPTION 105-ALIGNMENT IN PAVEMENT
NPT NRTH-UNDERGROUND ALIGNMENT
NRTH SECTION-STA 323+40 TO STA 323+70
DATE: 04/2017

DES: MRR | CHK: TJB
DRW: MRR | APR: TMH
TOWN: STEWARTSTOWN

TRANSMISSION LINE:
NRTH

EXHIBIT A