

**Exception Request No.: 175**  
**Section: WMNF**  
**Town: Woodstock**  
**Highway: US 3, D.W. Highway (Tier 2)**  
**Station: 1484+60± TO 1486+75±**  
**Drawing No.: WMNF C191-C192**  
**Survey Report Cross Reference No.: WMNF C191-C192**

**Exception Type: Alignment in Pavement**

#### Traffic Information

NHS: No  
ADT: 2400  
Traffic Control Type: Alt 1-Way  
Traffic Control Duration: Traffic control duration is estimated to be 6 days for the proposed installation, during which one lane will be closed.

#### Summary of Justification for Exception

NPT is requesting an exception from the UAM guidelines for the location of the cable trench in the pavement on US 3, Daniel Webster Highway from approximately station 1484+60 TO 1486+75 of the NPT WMNF Underground Alignment, sheet WMNF C191 and C192. The alignment is constrained by utilities and guardrail on the east side of US 3 and steep slopes and utility pole on the west side of US 3. The attached Exhibit A has been provided to demonstrate that construction behind the utility poles and guardrail would require work beyond the NHDOT ROW.

#### Technical Discussion of Justification of Exception

The alignment is constrained by utilities and guardrail on the east side of US 3 and steep slopes and utility pole on the west side of US 3. Immediately south of Parker Ledge Road, the slope rises steeply from the edge of the road. This steep slope and the existing utility pole prevent the alignment from being moved out of the pavement. Due to the limited ROW space and the steep slopes, relocating the utility pole away from the road is not possible. Similarly, the steep rock slope that extends up to Parker Ledge Road does not allow for regrading of the slope.

The guardrails and slopes on the east side prevent the alignment from being installed outside of the pavement on the east side. Additionally, the proposed alignment in Exhibit A eliminates two highway crossings that were proposed in the original permit submission, and reduces the depth of the crossing under the 48" box culvert at 1484+00±.

#### Impacts

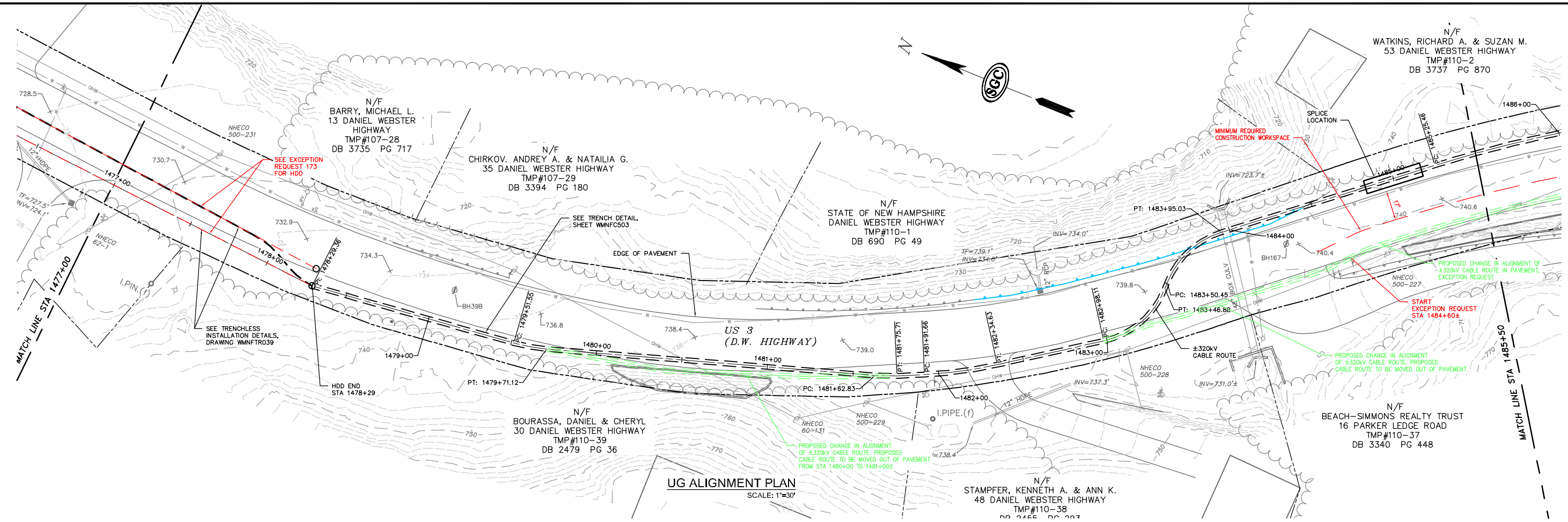
The design, as proposed, will not adversely affect the design, construction, stability, traffic, safety, environmental commitments, maintenance, or operation of the highway. The alignment has been located to minimize the disturbance of pavement. The installation of the duct bank and pavement restoration will be designed and constructed in accordance with conditions outlined in the NHDOT's

April 3, 2017 letter to the New Hampshire Site Evaluation Committee. The installation's proposed depth meets NHDOT's criteria relating to the structural box to minimize any potential conflicts with maintenance and future highway projects. A traffic control plan has been submitted to the NHDOT for this design and complies with the Manual on Uniform Traffic Control Devices.

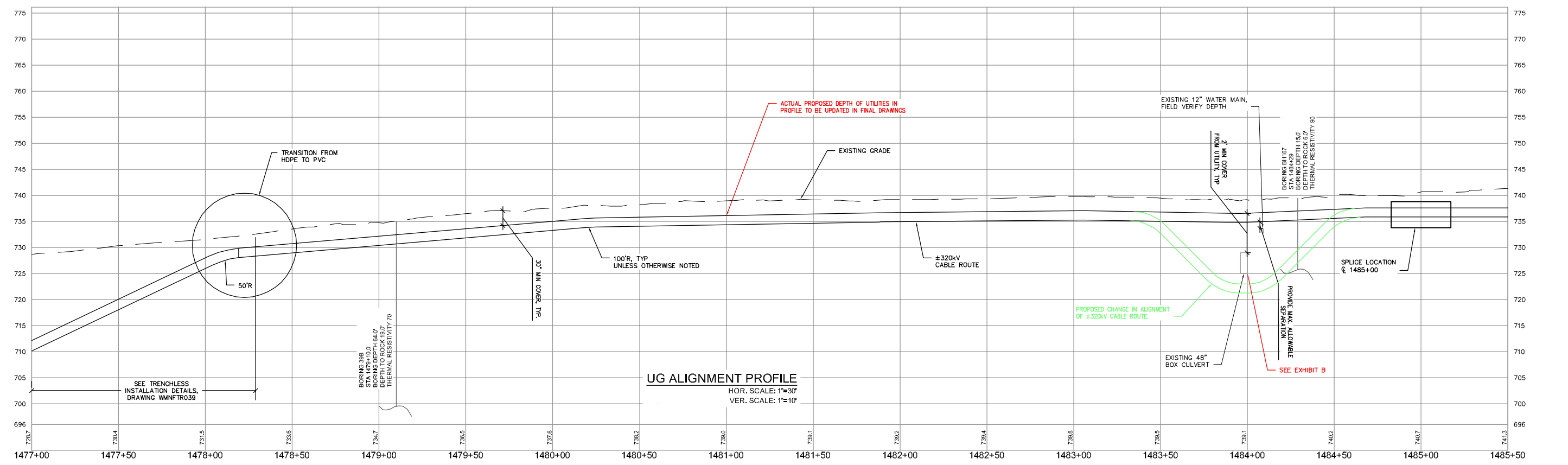
Supporting Documentation

See attached Exhibit A showing a plan and profile views.

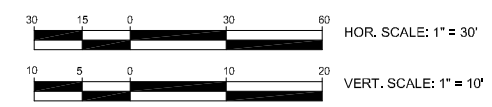
**PRELIMINARY - NOT FOR CONSTRUCTION**



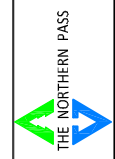
**UG ALIGNMENT PLAN**  
SCALE: 1"=30'



**UG ALIGNMENT PROFILE**  
HOR. SCALE: 1"=30'  
VER. SCALE: 1"=10'



NO.	REVISION	DATE	BY	CHKD	APPROV.
0	EXCEPTION REQUEST	07/13/17	MMR	MMR	MMR



Transmission Business

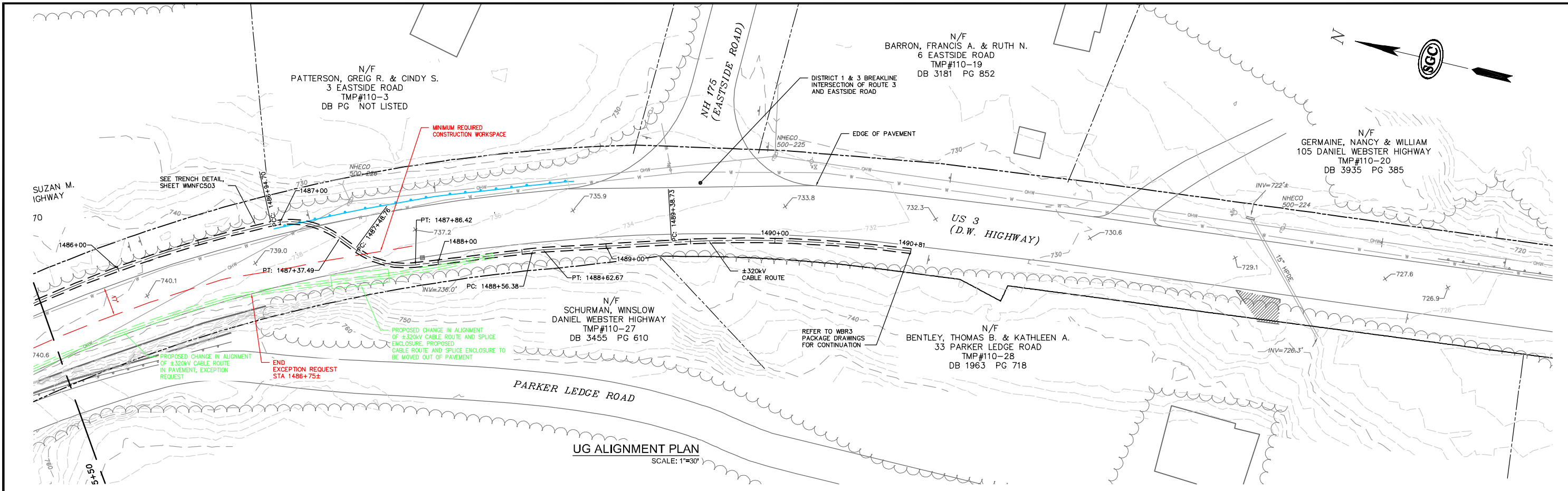
EXCEPTION 175-ALIGNMENT IN PAVEMENT  
NPT WMNF-UNDERGROUND ALIGNMENT  
WMNF SECTION-STA 1484+60 TO STA 1486+75  
SCALE: H. 1"=30', V. 1"=10'  
DATE: 07/20/17

DES: MRR CHK: TDG  
DRAW: MRR APR: TMT  
TOWN: WOODSTOCK

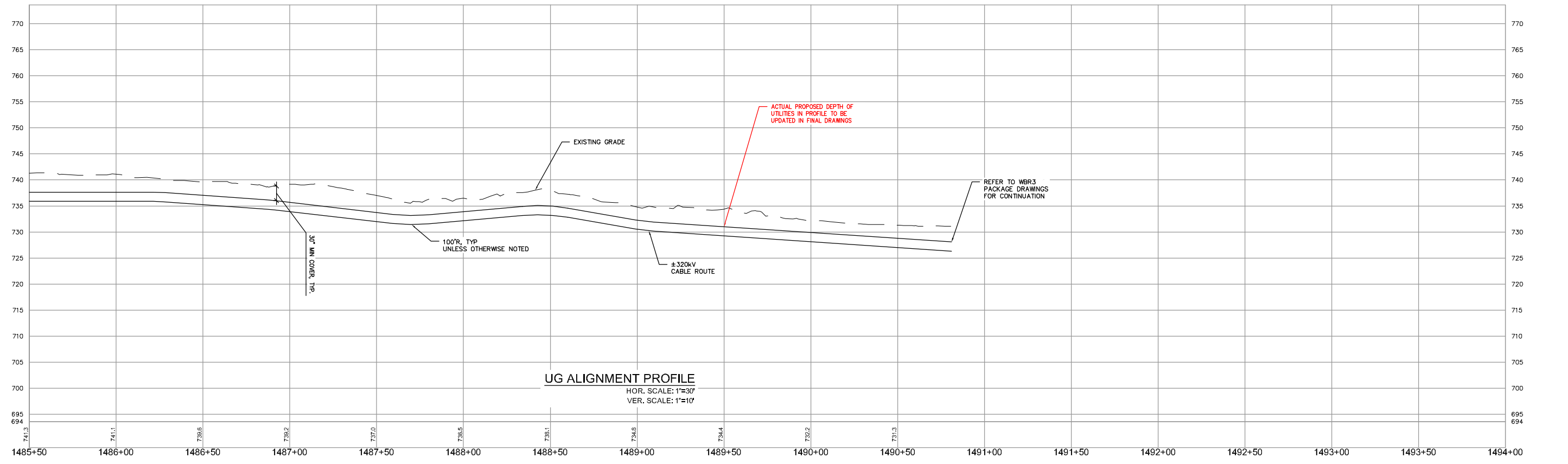
TRANSMISSION LINE:  
WMNF

EXHIBIT A.1

**PRELIMINARY - NOT FOR CONSTRUCTION**



**UG ALIGNMENT PLAN**  
SCALE: 1"=30'



**UG ALIGNMENT PROFILE**  
HOR. SCALE: 1"=30'  
VER. SCALE: 1"=10'



NO.	REVISION	DATE	BY	CHKD	APPRV.
0	EXCEPTION REQUEST	07/13/17	TDD	DOWN	TMH



Transmission Business

EXCEPTION 175-ALIGNMENT IN PAVEMENT  
NPT WMNF-UNDERGROUND ALIGNMENT  
WMNF SECTION-STA. 1486+60 TO STA. 1486+75  
SCALE: H. 1"=30', V. 1"=10'  
DATE: 07/20/17

DES: MRR CHK: TDD  
DRW: MRR APR: TMH  
TOWN: WOODSTOCK

TRANSMISSION LINE:  
WMNF

EXHIBIT A.2