

**Exception Request No.: 165**  
**Section: WMNF**  
**Town: Woodstock**  
**Highway: NH 112, Lost River Road (Tier 3)**  
**Station: 1262+75± to 1266+50±**  
**Drawing No.: WMNF C165 to C166**  
**Survey Report Cross Reference No.: WMNF C165 to C166**  
**Exception Type: Alignment in Pavement**  
**Highway Crossing**

#### Traffic Information

NHS: No  
ADT: 666  
Traffic Control Type: Alt 1-way  
Traffic Control Duration: Traffic control duration is estimated to be 12 days for the proposed installation, during which one lane will be closed.

#### Summary of Justification for Exception

NPT is requesting an exception from the UAM guidelines for the location of the cable trench in the pavement on NH 112, Lost River Road from approximately station 1262+75 to 1266+50 of the NPT WMNF Underground Alignment. (See Exhibit A).

Due to limited space outside the pavement and beyond the existing drainage infrastructure, before the steep slopes and mature tree line, construction outside the paved area from 1263+00± to the proposed highway crossing is not practicable. The proposed alignment is located beneath the pavement at a 5-foot offset from the catch basins to avoid future conflicts with drainage infrastructure repairs or replacement.

At approximately 1266+30 the alignment will be in the pavement to cross the road to take advantage of better terrain and the end of the guardrail on the west side of the road. This highway crossing will allow for more of the alignment to be installed beyond the pavement.

#### Technical Discussion of Justification of Exception

The roadway alignment at this location is constrained by drainage infrastructure on the east side of NH 112. Three existing catch basins are located off of the edge of the road on the east side. There are steep slopes, exposed bedrock and a mature tree line behind the catch basin. Construction on the back side would require benching into the slopes to create a level and safe working area that would disturb the existing mature trees and their root system.

NPT also evaluated placing the cable trench alignment on the west side of the road in this area, however there are existing guard rail and catch basin conflicts.

Excavation limits and work areas are shown on the attached drawings. During construction, one lane will remain open to traffic at all times.

Note: NPT is requesting an exception for the portion of the alignment from approximately station 1262+75 to 1266+50. In the original permit drawings, NPT proposed an alignment within the pavement for a longer portion of the roadway in this area. In response to NHDOT comments, NPT has reduced the length of the alignment within the paved area. The revised alignment will be reflected in revised drawings to be submitted at a later date.

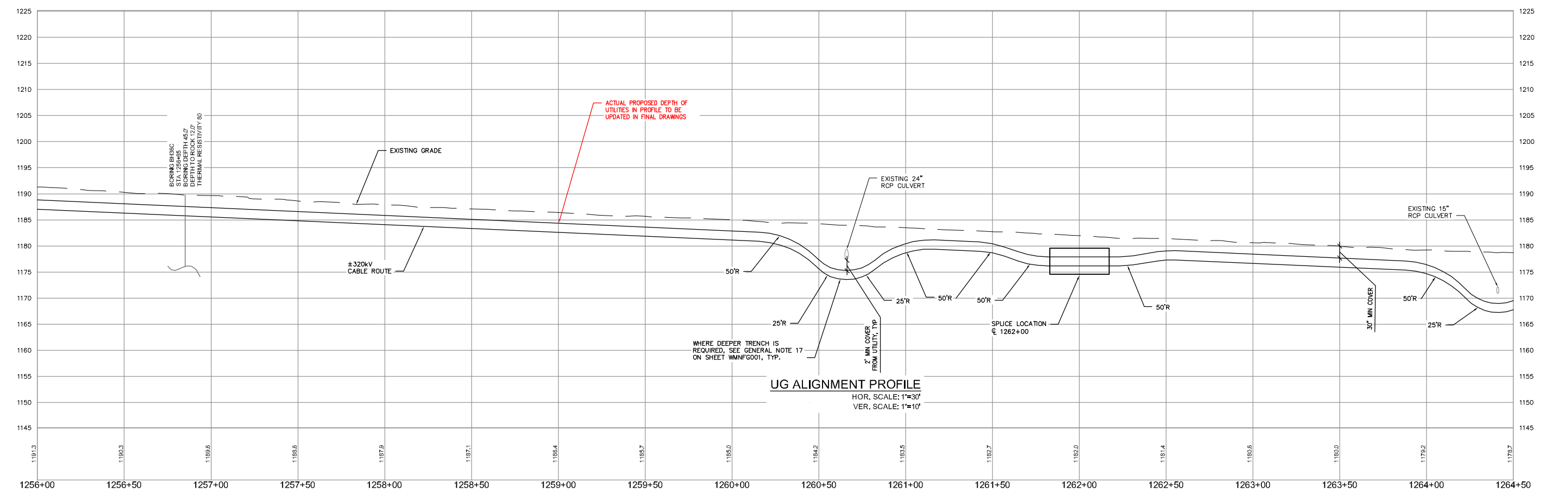
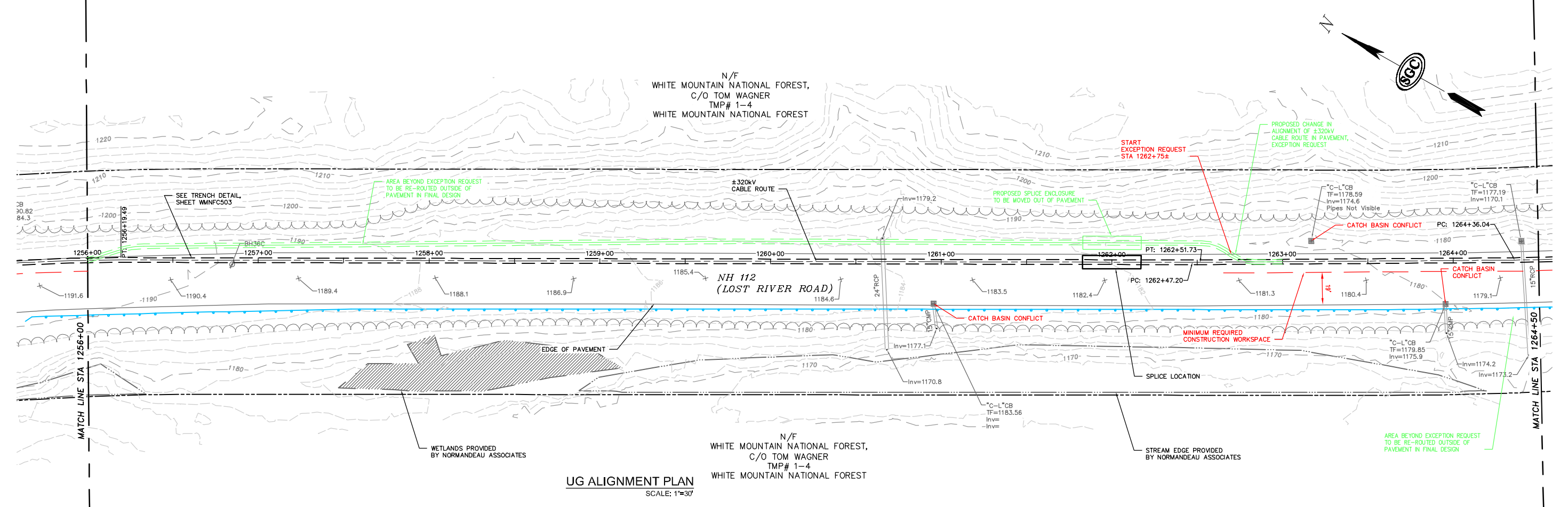
### Impacts

The design, as proposed, will not adversely affect the design, construction, stability, traffic, safety, environmental commitments, maintenance, or operation of the highway. The alignment has been located 5-feet off the edge of the catch basins to avoid future conflicts with drainage infrastructure repairs or replacement. The installation of the duct bank and pavement restoration will be designed and constructed in accordance with conditions outlined in the NHDOT's April 3, 2017 letter to the New Hampshire Site Evaluation Committee. The installation's proposed depth meets NHDOT's criteria relating to the structural box to minimize any potential conflicts with maintenance and future highway projects. A traffic control plan has been submitted to the NHDOT for this design and complies with the Manual on Uniform Traffic Control Devices.

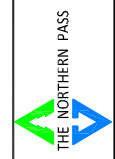
### Supporting Documentation

See attached Exhibit A showing a plan and profile view.

**PRELIMINARY - NOT FOR CONSTRUCTION**



NO.	REVISION	DATE	BY	CHKD	APPROV.
0	EXCEPTION REQUEST	07/17/17	FDD	MRR	TMH

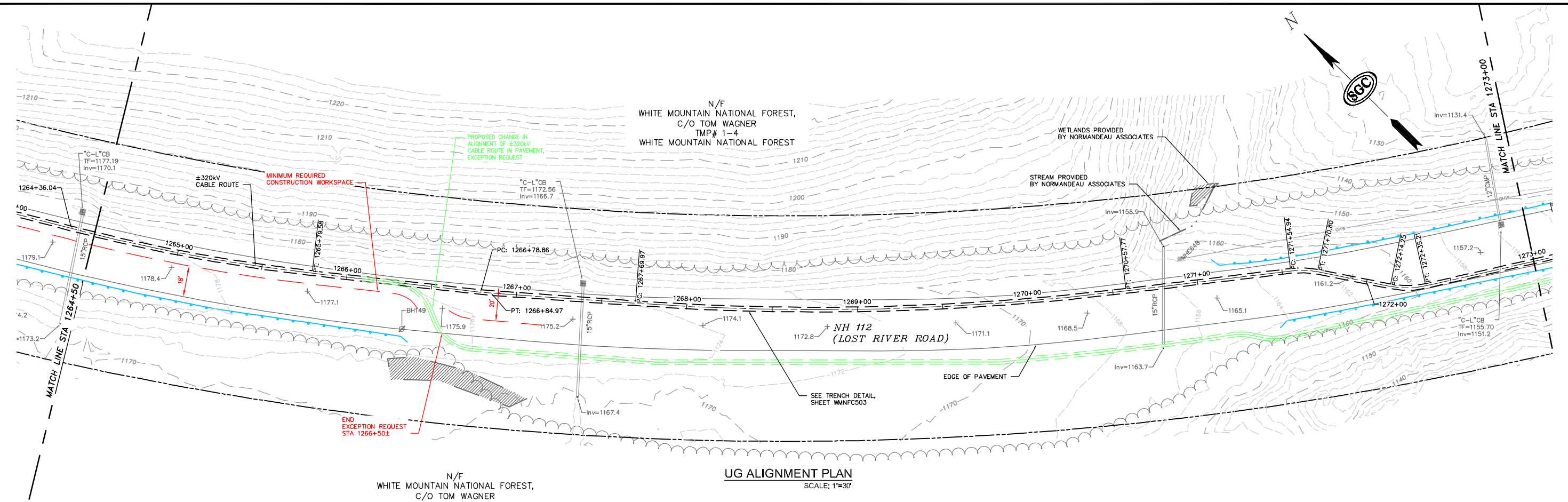


Transmission Business

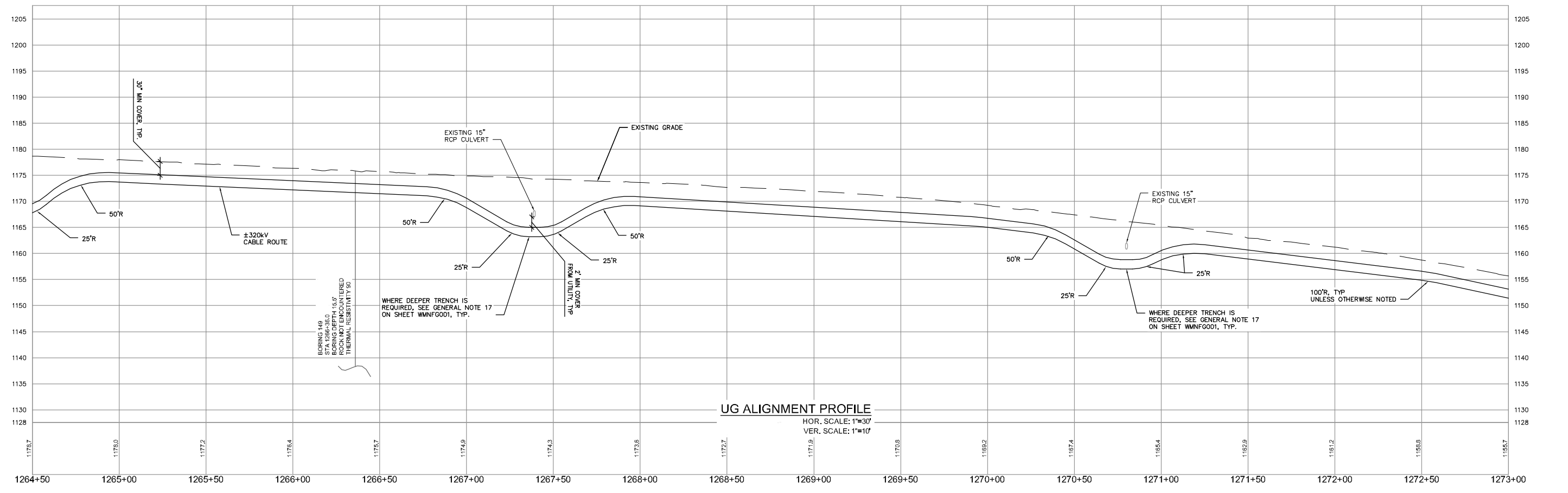
EXCEPTION 165-ALIGNMENT IN PAVEMENT  
NPT WMNF-UNDERGROUND ALIGNMENT  
WMNF SECTION-STA 1262+75± TO STA 1266+50±  
DATE: 07/20/17

DES: MRR CHK: TDD  
DRW: MRR APR: TMH  
TOWN: WOODSTOCK  
TRANSMISSION LINE:  
WMNF  
EXHIBIT A.1

**PRELIMINARY - NOT FOR CONSTRUCTION**



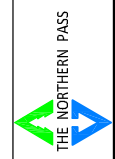
**UG ALIGNMENT PLAN**  
SCALE: 1"=30'



**UG ALIGNMENT PROFILE**  
HOR. SCALE: 1"=30'  
VER. SCALE: 1"=10'



NO.	DATE	BY	CHKD	APPROV.
0	07/17/17	TDD	DMR	DMF
		EXCEPTION REQUEST	DATE	REVISION



Transmission Business

EXCEPTION 165-ALIGNMENT IN PAVEMENT  
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SCALE: DATE: 07/20/17

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WMNF

EXHIBIT A.2