

Exception Request No.: 161
Section: WMNF
Town: Woodstock
Highway: NH 112, Lost River Road (Tier 3)
Station: 1223+50 to 1224+25
Drawing No.: WMNF C161
Survey Report Cross Reference No.: WMNF C161
Exception Type: Alignment in Pavement (Highway Crossing)

Traffic Information

NHS: No
ADT: 666
Traffic Control Type: Alt 1-way
Traffic Control Duration: Traffic control duration is estimated to be 6 days for the proposed installation, during which one lane will be closed.

Summary of Justification for Exception

NPT is requesting an exception from the UAM guidelines for the location of the cable trench in the pavement crossing NH 112, Lost River Road from station 1223+50 to 1224+25 of the NPT WMNF Underground Alignment. The proposed alignment is crossing the highway to stay out of the paved road as much as practical. The proposed alignment moves into the paved highway at this location to change sides of the road to avoid steep slopes and the guardrail to the south of the crossing on the east side and to avoid steep slopes and catch basins on the west side of the road to the north of the crossing. (See Exhibit A).

Technical Discussion of Justification of Exception

The proposed alignment crosses the roadway due to constraints on either side of the road. The alignment is located on the east side of the road north of the crossing to avoid the slopes, drainage structures and mature trees on the west side of NH 112. At 1223+50 the alignment crosses the road to the west side, to avoid the guardrail, steep slopes and mature tree line behind the guardrail, that starts at approximately, 1223+75 on the east side.

Excavation limits and work areas are shown on the attached drawings. During construction, one lane will remain open to traffic at all times.

Impacts

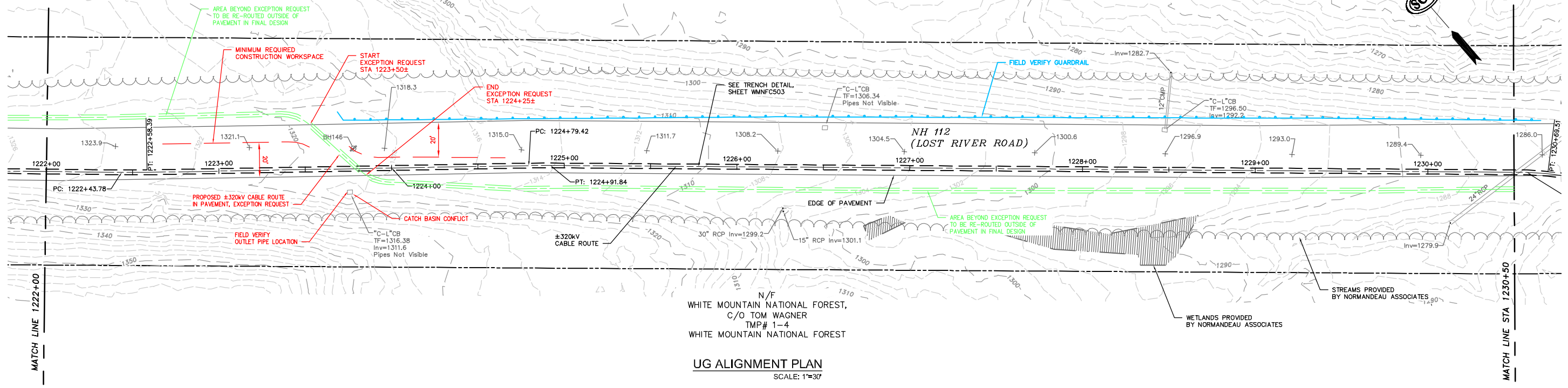
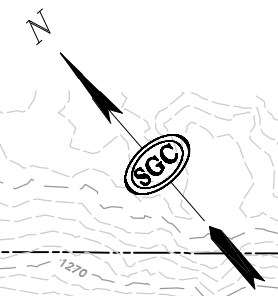
The design, as proposed, will not adversely affect the design, construction, stability, traffic, safety, environmental commitments, maintenance, or operation of the highway. The installation of the duct bank and pavement restoration will be designed and constructed in accordance with conditions outlined in the NHDOT's April 3, 2017 letter to the New Hampshire Site Evaluation Committee. The installation's proposed depth meets NHDOT's criteria relating to the structural box to minimize any potential conflicts with maintenance and future highway projects. A traffic control plan has been submitted to the NHDOT for this design and complies with the Manual on Uniform Traffic Control Devices.

Supporting Documentation

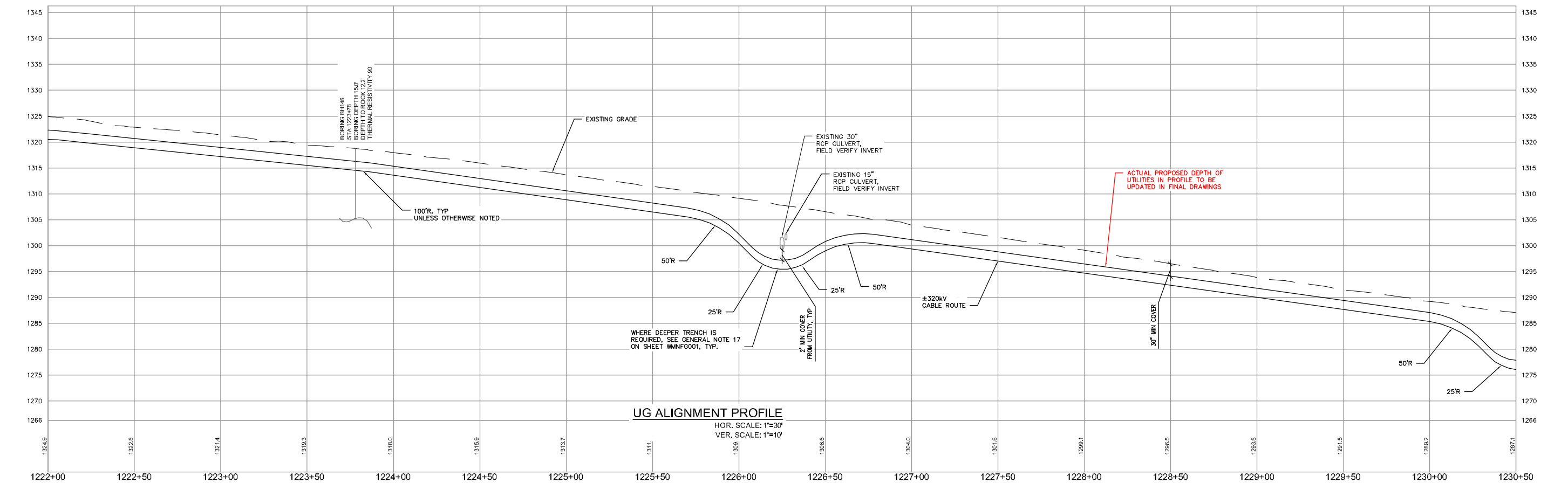
See attached Exhibit A showing a plan and profile view.

PRELIMINARY - NOT FOR CONSTRUCTION

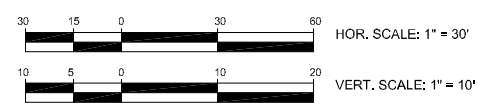
N/F
WHITE MOUNTAIN NATIONAL FOREST,
C/O TOM WAGNER
TMP# 1-4
WHITE MOUNTAIN NATIONAL FOREST



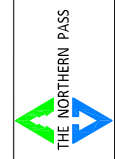
UG ALIGNMENT PLAN
SCALE: 1"=30'



UG ALIGNMENT PROFILE
HOR. SCALE: 1"=30'
VER. SCALE: 1"=10'



NO.	REVISION	DATE	BY	CHK	APPV.
0	EXCEPTION REQUEST	06/12/17			



Transmission Business

EXCEPTION 161-ALIGNMENT IN PAVEMENT
NPT WMNF-UNDERGROUND ALIGNMENT
WMNF SECTION-STA 1223+50± TO STA 1224+25±
SCALE: H. 1"=30', V. 1"=10'
DATE: 06/20/17

DES: MRR CHK: TDD
DRW: MRR APR: TMH
TOWN: WOODSTOCK
TRANSMISSION LINE:
WMNF
EXHIBIT A